

**EBURY
BRIDGE**
RENEWAL

HYBRID PLANNING APPLICATION
**PLANNING DEVELOPMENT
SPECIFICATION**

JULY 2020

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This report takes into account the particular instructions and requirements of our client.

It is not intended for and should not be relied upon by any third party and no responsibility is undertaken to any third party.

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Contents

	Page	
1	Introduction	1
1.1	Background	1
1.2	Document Purpose and Structure	1
2	The Application Site and Surrounding Area	3
2.1	The Site	3
2.2	Surrounding Area	5
2.3	Planning and Environmental Designations	5
2.4	Historical Designations	6
3	The Application	7
3.1	Description of Development	7
3.2	Proposal Summary	7
3.3	Matters for Approval and Matters Reserved	8
3.4	Application Documents	9
3.5	Design Documents for Approval	10
3.6	Primary Supporting Document	11
4	Development Fixes	13
4.1	Areas and Land Uses	13
4.2	Plans for Approval	25
5	Implementation and Delivery	28
5.1	Programme	28
5.2	Approach to Subsequent Consents	28

Appendices

Appendix A

Application Document List

Appendix B

Application Drawing List

1 Introduction

1.1 Background

This Planning Development Specification has been prepared by Ove Arup and Partners Ltd. ('Arup') on behalf of Westminster City Council's regeneration team in support of a Hybrid Outline Application for the renewal of the Ebury Bridge Estate, Ebury Bridge Road, London, SW1W 8PX ('the Site'). The Site falls within the administrative area of Westminster City Council ('WCC') and therefore the WCC Local Planning Authority ('WCC LPA') will determine the planning application.

Westminster City Council believes in building a City for All, where everybody can have a place they are proud to call home and as a result have embarked on a ambitious affordable housing programme. Key to this programme is the vision for the Ebury Bridge Renewal which is to set a new standard in estate regeneration. In partnership with Ebury Bridge residents, Westminster City Council is taking forward a scheme that seeks the comprehensive redevelopment of the existing estate. The scheme aims to create a vibrant, modern neighbourhood for both the existing community and new residents by:

- Delivering more affordable housing;
- Creating a sustainable mixed community;
- Delivering improved public spaces and community facilities;
- Partnering with residents and businesses in a meaningful and transparent way to put forward the best proposal for the Ebury Bridge Estate; and
- Setting the standard for estate renewal with high quality design throughout all tenure types.

Extensive consultation and engagement with a wide range of consultees, local stakeholders and residents to inform the proposals has been undertaken. Details on consultation and engagement are set out in the Statement of Community Involvement ('SCI').

1.2 Document Purpose and Structure

This Planning Development Specification sets out the elements of proposed development that define the basis of the submitted Hybrid Outline Application and its accompanying Environmental Statement ('ES').

This document should be read alongside the submitted Parameter Plans, and the Design Code chapter within the submitted Design and Access Statement. Together, these documents are the "Controlling Documents" that form the minimum and maximum parameters within which, future Reserved Matters Applications ('RMAs') must come forward.

The parameter approach has been set to ensure comprehensive masterplan strategies are delivered in a consistent manner while also ensuring each pursuant part of the Ebury Bridge Masterplan can come forward in future with sufficient

flexibility to respond to market and detailed design development. At the same time ensuring that the scheme remains within the envelope assessed as part of the application and its supporting ES.

This Planning Development Specification is structured as follows:

- Section 2 – describes the physical features of Application Site and surrounding area.
- Section 3 – provides a summary of the Hybrid Outline Application itself, the description of development, matters for approval and matters reserved, and the application deliverables.
- Section 4 – describes the development fixes that underpin the Masterplan.
- Section 5 – outlines the implementation and delivery programme of the Masterplan.

2 The Application Site and Surrounding Area

The Planning Statement and Design and Access Statement submitted as part of this Application provide a detailed description of the site and its surrounding context; alongside any relevant planning and environmental designations. The key elements of this are summarised below.

2.1 The Site

The Ebury Bridge Estate is located in the south west corner of the City of Westminster. As illustrated in Figure 1, it is bounded to the west by Ebury Bridge Road, to the north by Ebury Bridge, to the south by the recently constructed Grosvenor Waterside development and to the east by the railway line that connects to Victoria Station.

Figure 1: Existing Site Area



The original estate was constructed in the early 1930's and consisted of 9 blocks – with a further three blocks constructed in the late 1930's. Edgson House was subsequently completed in the 1950's and Wainwright House added in the 1980s. As a group therefore, heights across the estate range from 3 to 9 storeys, and comprised the following blocks:

- Rye House
- Westbourne House
- Bucknill House
- Victoria House
- Wainwright House
- Bridge House
- Pimlico House
- Mercer House

- Dalton House
- Hillersden House
- Wellesley House
- Edgson House
- Doneraile House

As illustrated in Figure 1 however, Cheylesmore House located in the south west corner of the estate is now held under a separate private freehold title so is no longer considered as part of the Site. Accordingly, the estate comprises 13 blocks accommodating 336 residential dwellings which are split into a mix of unit types as set out below in Table 1.

Table 1: Existing Unit and Habitable Room Split

	1 bed	2 bed	3 bed	4 bed	Total	GIA
Social Rented						
Units	84	85	20	9	198	11,352sqm
Hab Rooms	168	255	80	45	548	-
Private Leasehold						
Units	44	64	21	9	138	9,014sqm
Hab Rooms	88	189	84	50	411	-

Alongside the residential provision, 14 retail units with a total combined floorspace of 846sqm exist at ground floor level facing onto Ebury Bridge Road. These have a mix of A1-A3 Use Classes.

There has been a long-term aspiration to regenerate the estate since 2010 when the estate was first identified in WCC's Housing Renewal Strategy as one of five key estates within the borough in need of significant investment. Stemming from this, extensive consultation and a successful residents ballot culminated in the submission of a Full Planning Application in 2014 (LPA Ref: 14/01295/COFUL) for the estate's partial redevelopment and infill. This proposal was found to be unviable however, with the permission expiring in 2019 unimplemented.

In July 2017 attention turned back towards the development of more viable renewal options for the estate. Since this time, ongoing consultation with local residents culminated in the confirmation of the preferred renewal option being demolition and rebuild. Accordingly, WCC has since been undertaking a process of decanting residents from the remaining 13 buildings.

Edgson House, the tallest structure on the estate at 9 storeys, was one of the first buildings to benefit from vacant possession. With a view to providing a Meanwhile community space, café and workspace / retail units to serve as a new hub during the estate's regeneration, Edgson House was subsequently demolished. At the time of writing, the Meanwhile project was at an advanced stage of construction.

Edgson House aside, the ongoing decant has resulted in many units becoming empty and potentially subject to anti-social behaviour and maintenance issues. As such, the decision was taken to commence demolition of a number of buildings in advance of the submission of the Hybrid Outline Application for the estate's

redevelopment. The demolition of these buildings was subsequently approved via Prior Approval in October 2019.

2.2 Surrounding Area

In terms of surrounding land uses, whilst the immediate buildings are generally residential, wider uses are mixed comprising commercial, retail and (by virtue of the Site's proximity to Victoria Train and Coach Station) transport infrastructure.

The local built vernacular is diverse, comprising a variety of both period and modern architectural styles and typologies with no prevailing heights or characters. Whilst the period residential terraced properties along Ebury Bridge Road adjacent to the Site, constructed in the early 1800s, are 2 – 3 stories in height, the existing 1930's period mansion blocks both within and adjacent range from 4 – 9 storeys. The larger and more recent masterplanned residential-led development of Grosvenor Waterside to the south comprises buildings ranging in height from 7 – 14 storeys whilst the Chelsea Barracks proposal to the west, still under construction, was consented to include blocks up to 11 storeys in height. Both developments are residential-led and comprise a modern architectural vernacular.

As set out in section 2.4 below, there are no Statutorily Listed Buildings within the Site's red line. There are however a number within close proximity and scattered heavily to the Site's north and west. Similarly, the Site and much of its immediate proximity is not located in a conservation area. As illustrated in Figure 2, there are however a number nearby.

The Site has excellent connectivity, with a Public Transport Accessibility Level (PTAL) of 6b (which is the best). The Site is located within 800 metres of Victoria Underground Station (Victoria, Circle and District Lines) and Victoria Mainline Station; within 730 metres of Sloane Square Underground Station (Circle and District Lines) and within 550 metres of 10 bus routes. In addition to this, there is a TfL Cycle Hire Docking Station located on Ebury Bridge immediately adjacent to the Site to the north. This comprises 26 cycle spaces.

2.3 Planning and Environmental Designations

Whilst the Westminster City Plan 2016 ('Current WCP') and the Saved Unitary Development Policies 2010 ('UDP') form part of Westminster's current adopted Development Plan, the Draft City Plan 2019-2040 ('Draft WCP') is intended to supersede both documents. To date, the Draft WCP has undergone public consultation and was submitted to the Secretary of State ('SoS') for examination on 19th November 2019.

At the time of writing the examination is in process, but given its advanced stage of preparation and the fact it sets out the "direction of travel" for Westminster, the Draft WCP will be used in this case to assess the Site's designations. In this respect, the Site has the following designations:

- Located within the Central Activities Zone (CAZ);
- The Ebury Key Development Site;

- Housing Renewal Area;
- The ground floors of Rye House and Bucknill House fronting onto Ebury Bridge Road are part of the Ebury Local Centre;
- Crossrail 2 Safeguarding Zone across the north-western corner of the Site;
- Area of Play Space Deficiency; and
- Flood Zone 3.

In addition to the above, the Site is immediately adjacent to the following:

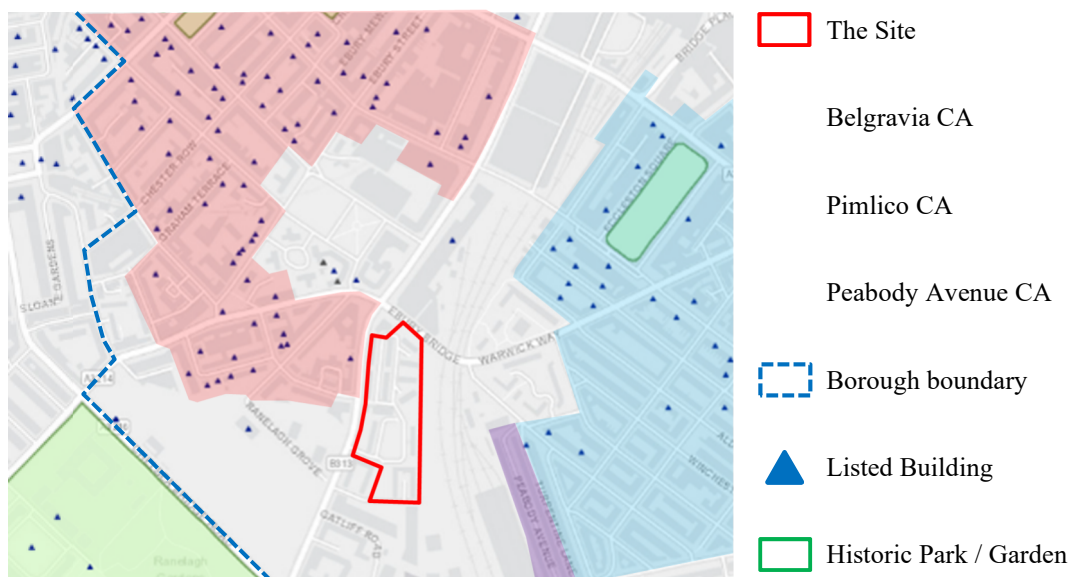
- The Victoria Opportunity Area to the Site's north;
- The Rising Sun public house and former garage adjacent to the junction of St Barnabas Street and Ebury Bridge Road are part of the Ebury Local Centre to the Site's west;

2.4 Historical Designations

There are no Statutorily Listed Buildings within the Application Site's red line. However, the 3-storey residential terraced properties opposite the Site (nos. 20 – 42 [even] Ebury Bridge Road) are Grade II Listed. Beyond this and set out in more detail in Figure 2, Listed Buildings and Historic Parks / Gardens are scattered heavily across the built environment in all directions around the Site – with the nearest Grade I Listed Building to the Site being the Church of St Barnabas circa 160 metres away.

The Site itself is not located in a designated conservation area. As illustrated in Figure 2 however, there are a number of conservation areas in very close proximity which should be a consideration. In addition to this, the Site is also located in an Archaeological Priority Area (Tier III).

Figure 2: Listed Buildings and Conservation Areas within Site Context



3 The Application

3.1 Description of Development

The description of the development for which planning permission is being sought ('the Proposed Development') is as follows:

- A mixed use development in outline for residential floorspace and ancillary residential facilities (Class C3) non-residential floorspace comprising flexible retail (Classes A1 – A4), community (Class D1), leisure (Class D2) and workspace (Class B1) floorspace; provision of basement; new pedestrian and vehicular access; and associated amenity space, open space, plant, landscaping, car and cycle parking, refuse storage, servicing area, and other associated infrastructure works; and
- Detailed planning consent for Blocks 7 and 8 comprising residential floorspace and ancillary residential facilities (Class C3); provision of a basement; new pedestrian and vehicular access; and associated amenity space landscaping, car and cycle parking, refuse storage, servicing area, and other associated infrastructure works.



3.2 Proposal Summary

This Application is being submitted as a Hybrid Outline Proposal – which comprises elements in part-detail (the Detailed Area) and part-outline (the Outline Area). The demises of each of these respective areas are shown on the Development Plot Plan (Parameter Plan Ref: EBE-AST-XX-XX-DR-A-011100),

whereby the orange hatched area pertains to the Outline Area and the unshaded area outlined in green relates to the Detailed Area.

The Detailed Area is being submitted in full whilst the Outline Area is being submitted with all matters reserved. Notwithstanding this, an illustrative proposal has been compiled across the Outline Area to demonstrate one way in which the Outline components could come forward in future in line with the parameters set out in this Development Specification. Both the Outline Area and the Detailed Area combined form “the Ebury Bridge Masterplan” – which as above, is currently illustrative.

3.3 Matters for Approval and Matters Reserved

The layout, scale, access and appearance of the Detailed Area of the Proposed Development are being submitted in full detail and include plans, elevations and section drawings for approval. The final landscaping and material specifications proposed however are to be agreed via planning condition.

The remaining parts of the Proposed Development comprise the Outline Area and is being submitted with all matters reserved. In this respect, parameter plans and supporting documents are being submitted for the Outline Area to underpin the principles of its future development. For clarity, Appendix 2 of this Development Specification provides a full list of all plans being submitted for approval.

Article 2 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 (‘DMPO’) sets out the requirements for outline planning applications. The following are “reserved” for later determination as part of future Reserved Matters Applications (‘RMAs’):

- **Means of Access** – As defined in the DMPO as “the accessibility to and within the site, for vehicles, cycles and pedestrians in terms of positioning and treatment of access and circulation routes and how these fit into the surrounding access network” is reserved.
- **Layout and Scale** – As defined in the DMPO, “the way in which buildings, routes and open spaces within the development are provided, situated and orientated in relation to each other and to buildings and spaces outside the development” [layout] and “the height, width and length of each building proposed within the development in relation to its surroundings” [scale] are reserved.

In this respect, both this Development Specification and the parameter plans listed in Appendix 2 are being submitted for approval and set the maximum parameters within which RMAs will be brought forward. On the grant of this Hybrid Outline Permission, it is anticipated that conditions will be imposed requiring the layout proposed under any RMAs to be in full accordance with these documents.

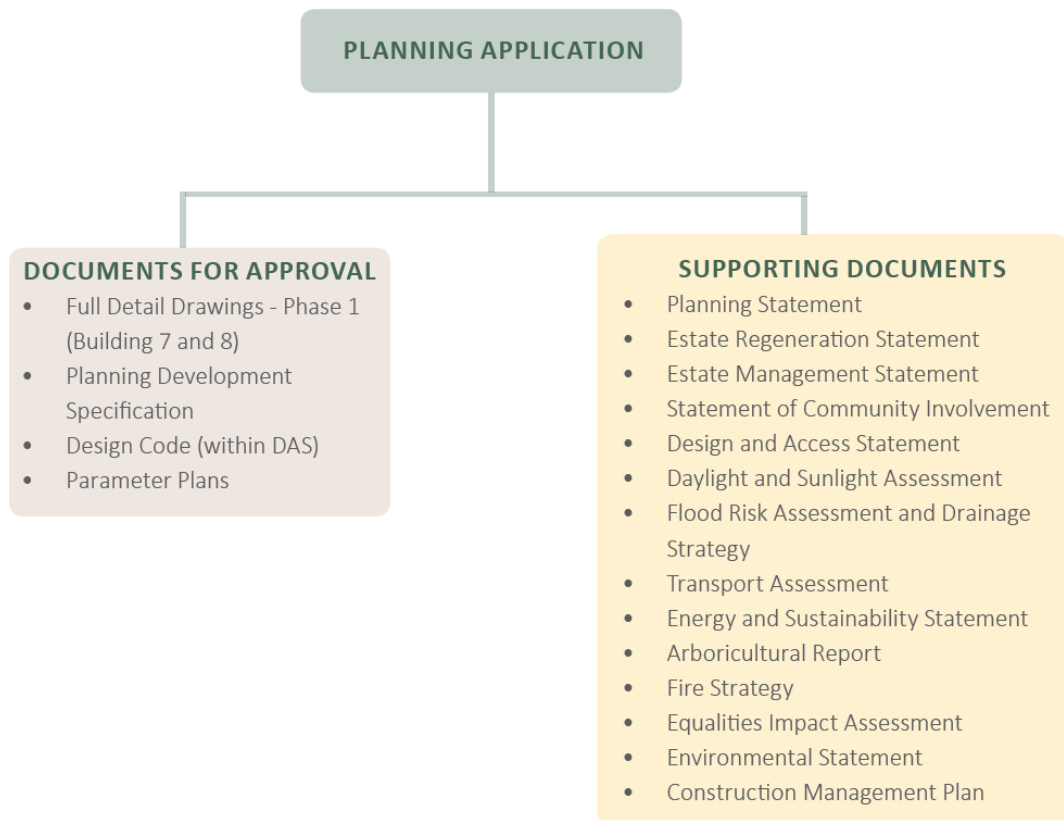
- **Appearance and Landscaping** – As defined in the DMPO, “the aspects of a building or place within the development which determines the visual impression the building or place makes, including the external built form of the development, its architecture, materials, decoration, lighting, colour and texture” and “the treatment of land (other than buildings) for the purpose of enhancing or protecting the amenities of the site and the area in which it is situated” are reserved.

The Architectural and Landscape Design Code submitted with this Application, which sits as Chapters 4 and 5 in the Design and Access Statement respectively, provides the guidelines within which the detailed design of the Outline Area must come forward. It is anticipated that compliance with the Design Code chapters will be secured via planning condition.

Save as set out above, the Ebury Bridge Masterplan being submitted as part of this Application is purely illustrative – the details of which are set out in the submitted Design and Access Statement. The Masterplan represents “design intent” and serves as a demonstration of how a holistic development could be brought forward fully in accordance with the Design Code and Parameter Plans. Whilst this approach allows the decision-maker control over the scope and content of subsequent RMAs, it more fundamentally also offers a degree of flexibility in any final RMA design. This allows said design to evolve in response to site conditions prevalent at the time of implementation.

3.4 Application Documents

Submitted with this Hybrid Outline Application are a range of documents, some of which are being submitted for approval and others of which are being submitted in support. Whilst a comprehensive list of documents being submitted with this Application is set out in Appendix A, a summary of, and relationship between these documents is illustrated in Figure 3, with the specific purpose of those that are relevant explained in more detail below.

Figure 3: Relationship between Application Documents

3.5 Design Documents for Approval

The Detailed Area Application Drawings (Full Detailed Drawings – Phase 1 (Building 7 and 8) aside, the documents listed in part a) of Figure 3 are the “Controlling Documents” for the Outline Area component of this Hybrid Outline application. These will inform and control the detailed design of the phases within this area as they are progressed through RMAs. The specific role of these documents are set out below:

- **Planning Development Specification**

Specifically, Section 4 of this document sets out the fixes and thresholds for both the proposed uses and the quantum’s of development.

- **Design Code DAS Chapters 4 and 5 (part)**

The identified Design Code pages within the Design and Access Statement Chapters 4 and 5 form the Architectural and Landscape Design Code. This provides a common set of design principles and written rules upon which all future RMAs will be based.

The Design Code is multi-level, providing details both specific to each Development Plot proposed across the Outline Area (Blocks 1-6 and Block 9) – such as massing, appearance, and façade details; alongside more overarching site-

wide principles – such as wider layout, access / entry points, and locations of external amenity.

Where appropriate, the Design Code references and sits alongside images of the illustrative Masterplan. This is to demonstrate how the Design Code could theoretically be applied, and would not otherwise require RMAs to be developed identically.

- **Parameter Plans**

The Parameter Plans establish the key design parameters against which future RMAs for the Outline Area will be submitted and should be read in tandem with the Design Code. The list of Parameter Plans is provided in full at Appendix 2 of this document, with a further description of each contained in Sections 4.2.2.1 – 4.2.2.8 below. In summary however, the Parameter Plans identify:

- Development plot distribution;
- Site levels;
- Horizontal and vertical plot limits (at basement, ground, typical and upper floors);
- Land uses (at basement, ground, first, typical, and upper floors);
- Façade character by vertical span;
- Access and circulation;
- Public realm and amenity spaces (at ground, first and roof levels).

Compliance with the Parameter Plans will be enforced by planning conditions attached to the Hybrid Outline Planning Permission. In this regard, the Parameter Plans pertain only to the Outline Area of the Application. Where information is provided on the Parameter Plans within the Detailed Area demise, this is for information only and seeks to demonstrate how the Detailed Area is otherwise compatible with the wider Ebury Bridge Masterplan principles.

3.6 Primary Supporting Document

A list of the supporting documents submitted with this application is set out in part b) of Figure 3, with Appendices A and B comprising a more detailed schedule of all documents and plans submitted both for approval and support.

For avoidance of doubt therefore, the following documents / design components are not Controlling Documents:

- **The Design and Access Statement (excluding Design Code pages)**

This document explains the vision for the renewal of the Ebury Bridge Estate and the design principles that have been applied to it. It demonstrates how both the Site's context and pre-application consultation with key stakeholders have influenced the approach to the development and, in turn, how site specific constraints have been addressed in the conception of the illustrative masterplan. In

addition to this, the Design and Access Statement also contains Computer Generated Images ('CGIs') which demonstrate as realistically as possible how both the Detailed Area and the Outline Area could look, if the latter was built out in full in accordance with the Design Code and Parameter Plans.

The Design and Access Statement is therefore not a Controlling Document for the Outline Area. It is however an important reference for future RMAs given that it provides a useful explanation of the background, context, rationale and approach. In this respect

- **The Illustrative Masterplan**

Details of and assessments based on the illustrative masterplan are referenced throughout the supporting documents noted in part b) of Figure 3. These are however theoretical, and set out to demonstrate how the Outline Area could be delivered in conformity with the Parameter Plans, Design Code and Development Specification Document. It is therefore not a Controlling Document.

- **Contextual Application Site Drawings**

Further details are set out in Appendix 2 of this document and, in summary, comprise a mix of plans illustrating existing layouts / levels; and elevations of the existing estate alongside elevations of the new Masterplan. These drawings are being submitted for information and do not otherwise dictate or influence the form or configuration of the Proposed Development. Accordingly, they are not Controlling Documents.

4 Development Fixes

This section identifies and establishes the parameters and fixes within which, RMAs pursuant to the Outline Area of the Hybrid Outline Application must be developed in accordance. In summary, these comprise (a) the locations and quantum of the proposed areas and land uses; and (b) the fixes established by the submitted plans themselves. It should be noted that the parameters and fixes set out are also the framework against which the ES assessment has been prepared.

In this respect, the Design Code is also important in so far as it contains a comprehensive set of rules to further inform future development (which are not repeated in this Development Specification Document). As noted above, the design of proposals within the Outline Area brought forward through future RMAs must comply with this Development Specification, the Parameter Plans and the Design Codes and will be secured via planning condition.

4.1 Areas and Land Uses

The maximum overall Gross Internal Floor Area (GIA) proposed for the Site is 56,617sqm. This figure comprises the floorspace within both the Detailed Area and the Outline Area.

4.1.1 Summary of Overall Areas and Uses

The maximum floorspace for each land use proposed is set out in Table 2 below.

Table 2: Proposed Maximum Floor Area

Land Use	Maximum Floorspace (GIA)*	Total Residential Floorspace
Detailed Area (Blocks 7 and 8)		
Residential Units	16,589sqm	16,949sqm
Residential Ancillary	360sqm	
Non-Residential	0sqm	
Outline Area		
Residential / Residential Ancillary /	36,650sqm	36,650sqm
Non-Residential	3,018sqm	

**Notes:*

All figures are in GIA format, and are those usually occupied by people i.e. habitable spaces. Therefore, plant rooms, car / cycle parking areas, ancillary stores, cores, stairwells, corridors and circulation routes, and spaces for on-site energy generation are excluded from these calculations. All floor area figures within the table are maximums, with the final total provision to not exceed these unless an amendment to this proposal is regularised in future.

Residential Ancillary includes premises and facilities which are accessible only by, and provide services specifically to residents, such as an Estate Management Office and liaison spaces.

In addition to the uses set out in Table 2, the Masterplan will also include the following:

- Open Space, in the form of publicly accessible, communal private and private;
- Play space for children aged 0-4, 5-11 and 12+;
- The removal of existing trees to be replaced with a comprehensive tree planting strategy as part of the landscaping scheme;
- The provision of car parking for use by residents who are “White Badge” holders, alongside two Car Club spaces for use by all residents.
- The provision of cycle parking - long stay bays for residents in the form of integral cycle stores; long stay bays for use by staff of the retail units to be provided within the demises of the retail units themselves; and short stay visitor bays in the public realm.

More detail on each of these areas and uses is provided in the below sections 4.1.2 – 4.1.13.

4.1.2 Distribution of Areas and Uses

The overall floor area is proposed and divided across nine development plots which define the distribution of development and main uses within the Site. The development plots are configured to create a cohesive network of open spaces (which includes 4 public squares), connected from north to south to facilitate legible walking and cycling routes. This is alongside new pedestrian and vehicle routes from Ebury Bridge Road into the development itself.

The configuration of the development plots proposed as part of the Outline Area is controlled by the Parameter Plans. Specifically, the Development Zone Plans (Parameter Plan Refs. EBE-AST-XX-XX-DR-A-01110-14) which fixes the maximum demises and extents of each of the seven Outline Development Plots from basement level to upper floor level. These plans also illustrate (for information purposes) the location and extents of the Detailed Area which includes Blocks - 7 and 8.

The total proposed development floor area across the Masterplan is divided between the Outline Area and Detailed Area, in accordance with Tables 3 and 4 below.

4.1.2.1 Detailed Area

The Detailed Area of the Hybrid Outline Application seeks permission for 16,949sqm GIA within Blocks 7 and 8. There is no non-residential floorspace proposed here – only residential. The distribution of this floorspace per block is set out in Table 3.

Table 3: Detailed Area - Floor Area by Block*

Building	Residential Units	Residential Ancillary	
Block 7	8,126sqm	360sqm	
Block 8	8,463sqm	-	
Total	16,589sqm	360sqm	16,949sqm

***Notes:**

All figures are in GIA format, and are those usually occupied by people i.e. habitable spaces. Therefore, plant rooms, car / cycle parking areas, ancillary stores, cores, stairwells, corridors and circulation routes, and spaces for on-site energy generation are excluded from these calculations. Residential Ancillary includes premises and facilities which are accessible only by, and provide services specifically to residents, such as an Estate Management Office and liaison spaces. All floor area figures within the table are maximums, with the final total provision to not exceed these unless an amendment to this proposal is regularised in future.

4.1.2.2 Outline Area

The Outline Area of the Hybrid Outline Application will be delivered as a phased, mixed-use development across seven blocks, comprising a maximum GIA of 39,668sqm. Table 4 below indicates how this maximum GIA will be distributed by block.

Table 4: Outline Area - Maximum Floor Area by Development Plot*

Development Plot	Residential Units	Residential Ancillary	Non-Residential		
Block 1	36,610sqm	40sqm	1,600sqm		
Block 2					
Block 3					
Block 4				508sqm	
Block 5				-	
Block 6				910sqm	
Block 9					
Total	36,610sqm	40sqm	3,018sqm	39,668sqm	

***Notes:**

All figures are in GIA format, and are those usually occupied by people i.e. habitable spaces. Therefore, plant rooms, car / cycle parking areas, ancillary stores, cores, stairwells, corridors and circulation routes, and spaces for on-site energy generation are excluded from these calculations. All floor area figures within the table are maximums, with the final total provision to not exceed these unless an amendment to this proposal is regularised in future.

Residential Ancillary includes premises and facilities which are accessible only by, and provide services specifically to residents, such as an Estate Management Office and liaison spaces.

As noted in Table 4, whilst the non-residential floorspace is apportioned by block or groups of blocks, the maximum residential GIA is not. This will allow the final residential GIA, dwelling mix and tenure type of each block to crystallise as part of future RMAs with full flexibility within the built extent set by the Parameter Plans.

4.1.3 Land Uses per Development Plot

The principal and other allowable land uses within each development plot proposed as part of this Hybrid Outline Application are set out below in Table 5.

Table 5: Allowable Land Uses across the Proposed Development

Development Plot	Land Use
Block 1	<p>Principal land use: Residential (C3), including ancillary facilities.</p> <p>Other land uses: Retail(A1-A4); community and cultural uses (D1). In this respect, no more than 460sqm to be A3, no more than 340sqm to be A4, and no more than 150sqm to be D1.</p>
Block 2	
Block 3	
Block 4	
Block 5	<p>Principal land use: Residential (C3), including ancillary facilities.</p> <p>Other land uses: 350sqm of commercial uses (B1); 158sqm of community and cultural uses (D1).</p>
Block 6	Principal land use: Residential (C3), including ancillary facilities.
Block 7 (Detailed Area)	Principal land use: Residential (C3), including ancillary facilities.
Block 8 (Detailed Area)	Principal land use: Residential (C3), including ancillary facilities.
Block 9	<p>Principal land use: Residential (C3), including ancillary facilities.</p> <p>Other land uses: Community, cultural or leisure uses (D1/D2); restaurant or café uses (A3). In this respect no more than 130sqm to be A3.</p>

All spaces included are those usually occupied by people i.e. habitable spaces. Therefore, plant rooms, car / cycle parking areas, ancillary stores, cores, stairwells, corridors and circulation routes, and spaces for on-site energy generation are excluded from these provisions.

4.1.4 Residential Uses

This Hybrid Outline Application seeks permission for a maximum level of residential floorspace of 53,599sqm. As cumulated in in Table 2, this comprises 36,650sqm of such floorspace in the Outline Area and 16,949sqm of floorspace in

the Detailed Area. These floorspace provisions comprise both residential and ancillary residential habitable accommodation.

To allow flexibility in future with regards to potential changes to plant requirements, parking, storage and energy generation, these spaces (defined as those not usually occupied by people) are excluded from the fixed maximum residential floorspace provision, although still controlled by the maximum built extent set by the Parameter Plans.

4.1.5 Dwelling Mix

The Masterplan presents a detailed dwelling mix for both the Detailed Area and Outline Area, devised based on current decant needs and market demands. However, Masterplan wide this dwelling mix is only indicative for the purposes of assessment, and demonstrates how the Parameter Plans and Design Code can be used to underpin a form of build out.

Notwithstanding, it is recognised that to provide certainty of the delivery of a mix of sizes moving forward, a fix is required pursuant to the minimum and maximum provision of each unit type. Accordingly, the Masterplan should deliver a total provision of dwellings across the size ranges set out in Table 6.

Table 6: Overall Residential Dwelling Mix Masterplan-wide

	Studios	1 bed	2 bed	3 bed	4 bed	5 bed
Unit Split %	0-5%	30-35%	40-50%	15-25%	0-5%	0.5%
				20% minimum		

In addition to the above dwelling mix, there is also a need to ensure a sufficient uplift specifically of family units. On the basis that the existing Ebury Bridge Estate comprises a total of 336 units as a baseline (set out in Table 1), of which 59 are family units, the Masterplan-wide uplift of units should be comprised of no fewer than a 22% provision of family units.

4.1.5.1 Outline Area

The proposed dwelling mix for the Outline Area (Blocks 1-6 and Block 9) will be compliant with Table 6 and will be provided by the Applicant as part of each RMA. They will be agreed in line with needs and trends as current at the time.

4.1.5.2 Detailed Area

Within the Detailed Area, the housing mix is fixed. In total, this area will deliver 226 new homes spread across Blocks 7 and 8 with the mix set out in Table 7. This is fully compliant with the ranges illustrated in Table 6.

Table 7: Dwelling Mix in Detailed Area

	Studios	1 bed	2 bed	3 bed	4 bed	5 bed	Total
Block 7 Units	0	35	49	26	2	0	112
Block 8 Units	0	33	51	25	4	1	114
Total Provision	0	68	100	51	6	1	226
Total Split %	0	30%	44%	22.5%	3%	0.5%	100%

4.1.6 Affordable Housing

As an estate regeneration scheme, the affordable housing strategy is twofold.

- a) Ensure the like for like reprovision, both on a habitable room and a floorspace basis, of all existing affordable units across the estate.
- b) Ensure the affordable quantum above this baseline reprovision (net uplift) is maximised.
- c) Ensure the tenure split supports the creation of a mixed and balanced community.

With regards to point a), Table 1 illustrates the existing residential provision across the existing estate. There are a total of 548 Social Rent habitable rooms which cover an area of 11,352sqm GIA – and no intermediate units. As a minimum therefore, Masterplan-wide there would should be the provision of at least 548 Social Rent habitable rooms across a minimum area of 11,352sqm GIA.

With regards to point b), the illustrative Masterplan proposes a net residential uplift of 1,223 habitable rooms, of which 669 (comprising Social Rent and Intermediate tenures) are affordable. This equates to a 55% provision. Across the illustrative Masterplan as a whole, a total of 2,182 habitable rooms are provided comprising 1,217 affordable - which equates to a 56% provision.

The Financial Viability Assessment ('FVA') submitted to support this Hybrid Outline Application recognises that the 55% uplift affordable provision and the 56% total affordable provision is currently the maximum viable that can be delivered and is therefore an interim fix as part of this proposal. It is recognised however that viability parameters may shift with future RMAs. A review of the FVA will therefore be submitted with every subsequent RMA to assess if this 56% affordable provision can still be adhered to viably or else if it must reduce. To provide certainty moving forwards however, both the affordable habitable room uplift and overall affordable provision will comprise a minimum baseline of 50%. This will be regardless of the outcome of future viability assessments.

With regards to point c), of those affordable habitable rooms across the Masterplan that are an uplift against the existing provision, 30-40% should be provided as Intermediate and 60-70% should be provided as Social Rent.

4.1.6.1 Outline Area

The affordable housing provision for the Outline Area (Blocks 1-6 and Block 9) will be provided by the Applicant as part of each RMA, and will be agreed in line with needs and trends as current at the time. As a minimum however, every RMA submission for residential blocks should maintain a masterplan wide level of 50% of new habitable rooms being affordable at completion of that phase in order to provide certainty of affordable delivery and distribution.

This will ensure both the social rent floorspace re-provision and the overall affordable uplift (and of this, the tenure split) can continue to be maintained in line with the fixes set out in Section 4.1.6.

4.1.6.2 Detailed Area

Within the Detailed Area, the affordable housing mix is fixed. In total, this area will deliver a total of 676 habitable rooms spread across Blocks 7 and 8 with a tenure split set out in Table 7.

Table 8: Affordable Provision in Detailed Area

	Block 7 Hab Rooms	Block 8 Hab Rooms	Total Hab Rooms	Hab Room % Split		
Social Rent	158	256	414	61%	525	79%
Intermediate	65	46	111	17%		21%
Market	108	43	151	22%		
Total	331	345	676	100%		

78% of the Detailed Area will comprise affordable habitable rooms.

4.1.7 Unit Sizes and Accessibility

Dwelling sizes across the entirety of the Masterplan will accord with the requirements of the Draft London Plan and be in line with the Technical Housing Standards – Nationally Described Space Standard. Final unit layouts will be devised through future RMAs, with the exception of the Detailed Area which includes fixed and final unit layouts as part of this submission.

As set out in the supporting Design and Access Statement and submitted Detailed Area Application Drawings, Blocks 7 and 8 within the Detailed Area comprise a range of different unit types and layouts which are considered appropriate to meet a variety of housing needs.

In line with the Draft London Plan and Building Regulations Parts M4(2 and 3), 10% of homes across the Masterplan will be delivered as “wheelchair units”. Precise details of these units will be agreed through future RMAs, though as a baseline all “wheelchair units” will be delivered as “wheelchair adaptable” with each blocking meeting the 10% provision.

4.1.8 Non-residential Uses

The Proposed Development will provide a maximum of up to 3,018sqm of flexible non-residential floorspace across the Masterplan. The Uses Plans (Parameter Plan Refs: EBE-AST-XX-XX-DR-A-011120-25) confirm the location of the non-residential land uses at basement, ground and first floor levels. These illustrate the majority of the non-residential uses fronting onto and wrapping around from Ebury Bridge Road – providing a continuous active frontage. The remaining non-residential uses are located towards the northern and southern gateways and within the main squares.

This is broken up into the following components:

- Up to 1,600 sqm of Class A1 - A4 / D1 floorspace where no more than 460sqm is to be A3; no more than 340sqm is to be A4; and no more than 150sqm is to be D1. This provision is to be located across Blocks 1-4 within the Outline Area.
- Up to 350sqm Class B1 floorspace and up to 158sqm Class D1 floorspace. This provision is to be located in Block 5 within the Outline Area.
- Up to 910sqm Class D1 / D2 / A3 floorspace where no more than 130sqm is to be A3. This provision is to be located in Block 9 within the Outline Area.

With the exception of Block 5 where the maximum demises of each use are to be fixed, the above provision allows for a range of uses, functions and unit sizes to provide accommodation for varying types of occupiers - ensuring maximum flexibility for units types to be delivered suitable to location and local and community need.

In all cases, the maximum floorspace provisions included in Table 5 for each land use within each development plot will not be exceeded.

4.1.9 Open Space and Amenity Space

The Hybrid Outline Application includes open space in the form of new streets, public squares, play areas, communal podiums and roof terraces, and private balconies and terraces. Amenity space across the Masterplan can be categorised into three key types; public; communal private, and private, set out in more detail below:

- **Public**

There is a total of 0.95ha of publicly accessible open space and public realm proposed across the Masterplan. Within the context of the wider 1.86ha Site, this equates to 51% of the Site and consists of the northern and southern gateways, the four public squares with their perimeter shared surfaces, and the connections westwards onto Ebury Bridge Road. Accordingly, the public open space accommodates a comprehensive landscaping strategy, semi-natural vegetation, amenity space and

children's play space, and wider opportunities for public interaction and gatherings. The location of the public realm across the Masterplan (both the Outline Area for approval and Detailed Area for information) is confirmed on the Ground Floor External Amenity Plan (Parameter Plan Ref: EBE-AST-XX-XX-DR-A-011160).

- **Communal Private**

The communal private amenity space proposed across the Masterplan takes the form of first floor podiums provided for Blocks 5 – 9 as confirmed on the First Floor External Amenity Plan (Parameter Plan Ref: EBE-AST-XX-XX-DR-A-011162); and roof terraces provided on the shoulder heights of all blocks across the Masterplan as confirmed on the Terraces External Amenity Plan (Parameter Plan Ref: EBE-AST-XX-XX-DR-A-011163).

Each podium level amenity space is accessible by residents of the buildings that flank it, meaning the majority of the podiums are available to two blocks each. They will contain doorstep playspace, and a mix of hard and soft landscaping to support flexible use by residents within adjacent buildings.

The roof terraces located on all blocks are only accessible to residents within that block itself. The layout of these spaces is to be kept deliberately simple, to comprise planting, seating areas, a pergola element and a table element as a focus for communal use.

Those residential units that are also located at these levels will be provided with defensible private terraces that are separate from the wider adjacent communal provision. In this respect, the final quantum of communal private space by block will be defined as part of future RMAs, given that they are dependent on the sizes of these private terraces – which in turn, is dependent on the sizes of the residential units they serve. Pursuant to Blocks 7 and 8 within the Detailed Area, the communal private provision is set out in more detail in section 4.1.9.2 below.

- **Private**

Private amenity space will be provided as defensible space in the form of terraces for ground floor units and those looking over a communal private provision; and balconies for upper units. The majority of units across the Masterplan will be provided with such space where constraints allow and these will be sized in accordance with the draft London Plan and Mayor's Housing Supplementary Planning Guidance ('SPG') requirements.

4.1.9.1 Outline Area

The approach to the different types of amenity space within the Outline Area will crystallise as part of future RMAs, in full compliance with the above noted Parameter Plans, relevant sections of the Design Code and the rationale highlighted in the DAS. Table 9 below sets out the quantum of amenity space to be provided across this area.

Table 9: Amenity Spaces Proposed in Outline Area

Amenity Type	Quantum	Form
Public	0.74ha	Final form and detail to emerge with future RMAs in line with the Parameter Plans and Design Code, and in line with the DAS rationale. This however includes three public squares, the northern and southern gateways and connections westwards to Ebury Bridge Road
Communal Private	To be confirmed with future RMAs	These will be provided across first floor podiums and roof terraces of Blocks 5-9; and roof gardens across Blocks 1-4. The final sizes of these spaces will be dependent on the final forms of the blocks themselves, though they will be designed and laid out in line with the rationale set out in the Design Code.
Private	Where provided per unit, they will be sized in accordance with draft London Plan Policy.	These will take the form of balconies and terraces, with the final provision being dependent on the mix and quantum of residential units proposed. Due to site constraints, it will unlikely be possible for every unit to be provided with private amenity space. In this respect, a robust justification will be provided.

4.1.9.2 Detailed Area

The Detailed Area crystallises the hierarchy of open spaces pursuant to Blocks 7 and 8 and one of the four proposed public squares. Accordingly, the layouts for each of these spaces are included as part of the submitted Detailed Area Drawing; with their design rationales included in the DAS. Table 10 below sets out the quantum and types of each amenity space type proposed.

Table 10: Amenity Spaces Proposed in Detailed Area

Amenity Type	Quantum	Form
Public	0.21ha	This will take the form of one of the four public squares alongside a pedestrian and vehicular access point onto Ebury Bridge Road. The square will comprise tree and shrub planting, informal amenity and meeting areas, and formal play equipment for children aged 4-10 years old. Whilst the square's perimeter will be accessible to vehicles, the nature of its shared "homezone" surface will prioritise pedestrians allowing it to make a contribution to the wider pedestrian-focussed public realm.
Communal Private	0.1ha	These will be provided across first floor podiums between Blocks 7 and 8 and north of Block 7. Blocks 7 and 8 will similarly be provided with roof terraces. On the podiums will be playspace and a mix of soft and hard landscaping; whilst the roof terraces will be more simply laid out with community tables, pergolas and seating areas.
Private	Where provided per unit, they will be sized in accordance with draft London Plan Policy.	These will take the form of balconies and terraces. Due to the constraints of the adjacent railway however, not all units on the eastern elevation will be provided with balconies. Residents will however have access to the level of amenity space noted above.

4.1.10 Play Space

As set out in the supporting DAS and Design Code, a total of 2,854sqm of play space is sought across the Masterplan which comprises 1,721sqm of play for children aged 0-4 years; 730sqm for children aged 5-11 years; and 403sqm for children aged 12+.

The structure, form and final size of each area of playspace that forms part of this total provision will crystallise as part of future RMAs. In this respect, within the Detailed Area, the podium deck will include 324sqm playspace for children aged 0-3; with a further 485sqm of playspace for children aged 4-10 provided in the adjacent public square.

4.1.11 Trees

As existing, there are a total of 32 trees (and smaller tree groups) within the development boundary and a further affected tree just beyond. Notwithstanding the tree off-Site, in order to facilitate the new Masterplan a maximum of 26 of the on-Site trees are to be removed with 6 to be retained. Table 11 below provides clarity into the categorisation of these trees.

Table 11: Existing Trees for Removal and Retention

	Existing Number	Number for Removal	Number for Retention
Category A	1	1	0
Category B	20	15	5
Category C	11	10	1
Total	32	26	6

As part of the landscaping proposal, a comprehensive tree planting strategy is proposed, which will ensure tree cover 50+ years into the future. The strategy will focus on the “right tree, right place” principles where all trees selected and planted will be as such to compliment the function and usage of their location. This will ensure an appropriate diversity of tree to assist with wayfinding, wildlife interest, visual interest, canopy coverage, and shading / cooling in summer.

Accordingly, and as noted in the Design Code, it is envisaged that alongside the 6 existing trees to be retained, a substantial tree planting strategy will be implemented across the Masterplan to ensure a net increase in tree numbers, biodiversity and amenity in the long term. In this respect, the exact numbers and locations of new trees to be planted across the Masterplan will emerge with future RMAs.

In this respect, of the 32 trees within the development boundary, seven (1 x Category A; 4 x Category B; 2 x Category C) are located in the Detailed Area. Of these and to be secured by the detailed consent, 2 Category B trees are to be retained with a further 47 proposed. This represents a tree coverage increase within the Detailed Area of 600% – from 7 as existing to 49 as proposed.

4.1.12 Parking

Car parking is to be provided in the form of disabled bays only for “White Badge” holders (with an outset provision commensurate with a minimum of 5.5% of units). As agreed with Transport for London during the pre-application discussions, given the constraints of the Site no passive disabled car parking bays are required. The car parking provision is to be provided at ground level beneath the podiums between Blocks 1-9, shielded from view of the wider public realm. The vehicular circulation routes to access these car parking areas are fixed by the Access and Circulation Plan (Parameter Plan Ref: EBE-AST-XX-XX-DR-A-011150).

Within the Detailed Area, parking is proposed beneath the podiums of Blocks 7 and 8 for 18 cars – of which, 2 will only be capable of use once the adjacent Block 9 comes forward. Alongside these, the final number of parking spaces within the Outline Area will be of a provision commensurate with the remaining capacity in order to ensure spaces for a minimum of 5.5% of residential units across the Masterplan. This remaining car parking provision will crystallise with the future RMAs for Blocks 5, 6 and 9.

In terms of car club, two spaces are proposed within the public realm. As these will be located within the Outline Area, again, they will crystallised with future RMA submissions.

In addition, cycle parking is proposed in accordance with draft London Plan standards which go beyond those of the current London Plan. Masterplan-wide, a total of 132 short stay spaces will be provided in the public realm comprising 31 for residential visitors and 101 for retail visitors. Long-stay spaces will be provided for the staff of the proposed retail units within the demises of the units themselves. The final numbers in this regard will be considered on a phased basis subject to future RMAs.

For residents, long stay cycle spaces will be provided in secure stores within each block, both at ground level and in the basement. The final provisions are subject to the precise residential mixes, hence will be considered on a phased basis subject to future RMAs. With this in mind, the final mixes within the Detailed Area necessitate the provision of 207 cycles for Block 7 and 212 cycles for Block 8. Of these, 5% will be provided for non-standard cycles.

4.1.13 Construction Management

A Construction Management Plan is submitted with this Hybrid Outline Application which sets out (i) in detail the envisaged demolition works and processes; and (ii) in high level the envisaged construction works and processes.

Accordingly, to ensure that these works are subject to local planning authority scrutiny (and unless an updated Construction Management Plan that contains the suitable level of detail pursuant to construction can be submitted in due time as part of this Application’s determination) it is envisaged that a condition will be attached to the Hybrid Outline Permission’s Decision Notice. It is expected that

such condition will require the submission of details for these works to the local planning authority for approval at the appropriate time.

4.2 Plans for Approval

4.2.1 Detailed Area Plans

A schedule of the detailed drawings submitted for approval in relation to the Detailed Area component of this Hybrid Outline Application is included at Appendix 2.

The detailed drawings comprise plans, elevations, sections, tenure plans and typical details of the Detailed Area of the Proposed Development which, in this case, comprises Blocks 7 and 8 and one of the four public squares proposed across the Masterplan.

The basement general arrangement plan illustrates the location of stores, plant and circulation in relation to the buildings above. The ground floor general arrangement plan indicates the location of the residential entrances, lobbies, cores and those apartments on the ground floor in relation to the wider Detailed Area landscaping strategy. The general arrangement plans for the subsequent floors above indicate the locations and demises of each apartment and their relationship with the circulation spaces.

A roof level plan is also included as part of the Detailed Area drawing pack. Although the roof articulation varies in this plan, its purpose is to illustrate the location of plant equipment and access alongside roof treatments.

In this respect, it should be noted that the general arrangement plans for all floors beyond the basement include those outline Development Plots in the immediate vicinity beyond the Detailed Area demise. This illustrates how the Detailed Area “dovetails” into the wider Masterplan context.

The elevation drawings provide north, east, south and west elevations of Blocks 7 and 8 together, which, alongside the sections and typical elevation drawings, illustrate the cumulative scale and appearance of the detailed component of the Hybrid Outline Application for approval.

The Detailed Area Drawing Pack should be considered together alongside the Design and Access Statement.

4.2.2 Parameter Plans

Table 12 (and Appendix 2 in more detail) lists the Parameter Plans that underpin the spatial fixes for the Outline Area of the Hybrid Outline Application. As illustrated in this table, whilst there are 24 individual Parameter Plans which are relevant to different aspects and floor levels across the Masterplan, fundamentally these can be grouped into eight Parameter Plan types. For these to be properly understood however, they must be read alongside the descriptions that follow below in paragraphs 4.2.2.1 – 4.2.2.8.

Table 12: Parameter Plan Numbers and Titles

Drawing Number	Title	
EBE-AST-XX-XX-DR-A-011100	Development Plot Plan	
EBE-AST-XX-XX-DR-A-011101	Proposed Site Levels	
EBE-AST-XX-XX-DR-A-011102	Masterplan Phasing	
EBE-AST-XX-XX-DR-A-011110	Development Zones	Basement
EBE-AST-XX-XX-DR-A-011111		Ground Floor
EBE-AST-XX-XX-DR-A-011112		Typical Floor
EBE-AST-XX-XX-DR-A-011113		Upper Floor (Level 6)
EBE-AST-XX-XX-DR-A-011114		Upper Floor
EBE-AST-XX-XX-DR-A-011115		Balconies
EBE-AST-XX-XX-DR-A-011120	Uses Plan	Basement
EBE-AST-XX-XX-DR-A-011121		Ground Floor
EBE-AST-XX-XX-DR-A-011122		First Floor
EBE-AST-XX-XX-DR-A-011123		Typical Floor (Level 3)
EBE-AST-XX-XX-DR-A-011124		Upper Floor (Level 6)
EBE-AST-XX-XX-DR-A-011125		Upper Floor (Level 12)
EBE-AST-XX-XX-DR-A-011130	Maximum Building Heights	
EBE-AST-XX-XX-DR-A-011140	Façade Character	Ground Floor
EBE-AST-XX-XX-DR-A-011141		First Floor
EBE-AST-XX-XX-DR-A-011142		Typical Floor (Level 6)
EBE-AST-XX-XX-DR-A-011143		Upper Floor Level 12)
EBE-AST-XX-XX-DR-A-011150	Access and Circulation	
EBE-AST-XX-XX-DR-A-011160	External Amenity	Ground Floor
EBE-AST-XX-XX-DR-A-011161		Podium
EBE-AST-XX-XX-DR-A-011161		Terraces

4.2.2.1 Development Plot Plan (011100)

The function of this plan is to define both the Outline Area planning boundary, and the locations and configurations of the Development Plots within. These plots represent the areas within which the building blocks would be situated.

4.2.2.2 Proposed Site Levels (011101)

The function of this plan is to set out the current and proposed maximum site levels at key points across the masterplan. This will ensure compatibility with the wider landscape and building design principles.

4.2.2.3 Masterplan Phasing (011102)

The function of this plan is to define the extents of each of the three phases envisaged for the Masterplan build out.

4.2.2.4 Development Zones (011110-011114)

The function of these plans is to define outer most demises of each Development Plot from basement level up to roof level. This delineates the maximum articulations proposed for each block across their heights.

4.2.2.5 Development Zones – Balconies (011115)

The function of this plan is to fix the minimum balcony widths across the Development. This will ensure the wider external design aesthetic of the Masterplan can be adhered to.

4.2.2.6 Uses Plans (011120-011125)

The function of these plans is to define the extent and demise of the residential and non-residential uses within the Development Plots, from basement level up through the height of each building.

4.2.2.7 Maximum Building Heights (011130)

The function of this plan is to define the maximum heights as Above Ordnance Datum (AOD) for each Development Plot across the Outline Area. Each plot comprises a number of shoulder heights which, for assessment purposes, have been assumed at their maximum. Thus development below these heights will be permitted where the proposals maintain compliance with the Design Code and other Parameter Plans.

4.2.2.8 Façade Character (011140-011143)

The function of these plans is to define the horizontal and vertical extents of the façade character zones proposed across the Outline Area. These plans should be read alongside the Design Code, which contains a detailed description of the design rationale and appearance principals for each zone.

4.2.2.9 Access and Circulation (011150)

The function of this plan is to define the nature and locations of access points and circulation routes into and through the development – alongside the type of user for whom they are to be designed.

4.2.2.10 External Amenity (011160-011162)

The function of these plans is to define, across each relevant level of the development, the extent of public open space in the form of public realm; the extent of communal private space in the form of podiums and roof terraces; and the locations of private amenity space in the form of terraces and balconies.

5 Implementation and Delivery

5.1 Programme

Demolition of some of the existing blocks across the Site is already underway, with prior approval for such works secured outside the scope of this Hybrid Outline Application. With this in mind, the overall development of the Site (subject to market conditions) is expected to continue to 2027 with handover and occupation of the final blocks to complete in 2028.

With a comparatively short programme, it is appreciated that the development will come forward in a manner whereby, at any point in time, multiple works are likely to be occurring across the Site. Accordingly, Table 13 below sets out the anticipated development programme of each set of proposed blocks. This is staged to ensure that required demolition of existing buildings and subsequent construction of new elements takes place in a timely, proper and orderly way that minimises disruption and conflict.

Table 13: Anticipated Development Programme

Aspect of the Proposed Development		Start date	Finish date
Construction of Detailed Area		Mid 2021	Mid 2023
Outline Area	Demolition for Blocks 1/5/6	Mid 2022	Early 2023
	Construction and fit out of Blocks 1/5/6	Early 2023	2025
	Demolition for Blocks 2/3/4/9	Late 2023	Early 2024
	Construction and fit out of 2/3/4/9	Early 2024	2027

5.2 Approach to Subsequent Consents

Implementation and delivery of the Proposed Development is expected to be controlled through planning conditions and, where appropriate, Unilateral Undertaking/Section 106 planning obligations. These are principally expected to relate to the following matters:

- Submission and scope of RMAs;
- Submission of additional details for approval that were not otherwise available at the time of submission of the Hybrid Outline Application
- Securing effective mitigation measures necessary to address impacts that are predicted to arise from the development;
- Establishing a set of triggers to ensure the appropriate provisions of the development are provided in a timely manner.

The parameters set out within this Hybrid Outline Application ensure an appropriate level of control over both the Detailed Area and the Outline Area. With particular regards to the Outline Area, all pursuant RMAs submitted for approval will be required to comply with the defined parameters provided by the

Controlling Documents, as explained in this Development Specification. This will ensure the Masterplan, as it evolves into the future, is complimentary to the Detailed Area.

Appendix A

Application Document List

A1 Application Documents List

The following documents are submitted with this Hybrid Outline Application. Submitted drawings aside, these documents read together, enable a full understanding of the proposal and a full assessment and evaluation of its features, impacts and benefits. They take into account procedural requirements and follow pre-application discussions with Westminster City Council.

Documents Submitted for Approval	
EBR-02	Planning Development Specification
EBR-05	Design and Access Statement (Design Code Chapters 4 and 5 part)
Document Submitted in Support	
-	Application Covering Letter
-	Application Forms and Certificates
EBR-01	Planning Statement
EBR-03-A	Estate Regeneration Statement
EBR-03-B	Estate Management Strategy
EBR-04	Statement of Community Involvement
EBR-05	Design and Access Statement
EBR-06	Daylight and Sunlight Report
EBR-07	Flood Risk Assessment and Drainage Strategy
EBR-08	Transport Assessment
EBR-09	Energy and Sustainability Statement
EBR-10	Arboricultural Report and Tree Survey
EBR-11	Fire Strategy
EBR-12	Equalities Impact Assessment

EBR-13	Environmental Statement and Non-Technical Summary (including Air Quality; Climate Change; Daylight and Sunlight; Ecology; Wind; Electro-Magnetic Interference; Health; Archaeology; Land Quality; Noise and Vibration; Socio-Economics; Heritage and Townscape Visual Impacts and Technical Appendices)
EBR-14	Construction Management Plan
EBR-15	Utilities Statement
EBR-16	Financial Viability Assessment

Appendix B

Application Drawing List

B1 Application Drawing List

The following drawing are submitted with the planning application. This table also notes if the respective drawings are for approval or submitted for information only.

Drawing Number	Title	Status
Site Plans and Masterplan (EBR-DRG-SMP)		
EBE-AST-XX-XX-DR-A-010000	Site Location Plan: Application Boundary	Approval
EBE-AST-XX-XX-DR-A-010001	Existing Site Plan: Application Boundary	Information
EBE-AST-XX-XX-DR-A-010010	Existing Site Levels	Information
EBE-AST-XX-XX-DR-A-010020	Demolition Plan	Approval
EBE-LBA-XX-GF-DR-L-300010	Illustrative Landscape Masterplan – Ground Floor Level	Information
EBE-LBA-XX-GF-DR-L	Illustrative Landscape Masterplan – Roof Level	Information
Existing and Proposed Site Elevations (EBR-DRG-SE)		
EBE-AST-XX-XX-DR-A-011200	Elevation West: Ebury Bridge Road – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011201	Elevation East: Railway – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011202	Elevation South: Grosvenor Estate – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011203	Elevation North: Ebury Bridge – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011210	Site Section West – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011211	Site Section East – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011212	Site Section South – Existing and Proposed	Information
EBE-AST-XX-XX-DR-A-011213	Site Section North	Information
Outline Parameter Plans (EBR-DRG-PP)		
EBE-AST-XX-XX-DR-A-011100	Development Plot Plan	Approval
EBE-AST-XX-XX-DR-A-011101	Proposed Site Levels	Approval
EBE-AST-XX-XX-DR-A-011102	Masterplan Phasing	Approval
EBE-AST-XX-XX-DR-A-011110	Development Zones – Basement	Approval
EBE-AST-XX-XX-DR-A-011111	Development Zones – Ground Floor	Approval
EBE-AST-XX-XX-DR-A-011112	Development Zones – Typical Floor	Approval

EBE-AST-XX-XX-DR-A-011113	Development Zones – Upper Floor (Level 6)	Approval
EBE-AST-XX-XX-DR-A-011114	Development Zones – Upper Floor	Approval
EBE-AST-XX-XX-DR-A-011115	Development Zones - Balconies	Approval
EBE-AST-XX-XX-DR-A-011120	Uses Plan – Basement	Approval
EBE-AST-XX-XX-DR-A-011121	Uses Plan – Ground Floor	Approval
EBE-AST-XX-XX-DR-A-011122	Uses Plan – First Floor	Approval
EBE-AST-XX-XX-DR-A-011123	Uses Plan – Typical Floor (Level 3)	Approval
EBE-AST-XX-XX-DR-A-011124	Uses Plan – Upper Floor (Level 6)	Approval
EBE-AST-XX-XX-DR-A-011125	Uses Plan – Upper Floor (Level 12)	Approval
EBE-AST-XX-XX-DR-A-011130	Maximum Building Heights	Approval
EBE-AST-XX-XX-DR-A-011140	Façade Character – Ground Floor	Approval
EBE-AST-XX-XX-DR-A-011141	Façade Character – Typical Floor (Level 3)	Approval
EBE-AST-XX-XX-DR-A-011142	Façade Character – Typical Floor (Level 6)	Approval
EBE-AST-XX-XX-DR-A-011143	Façade Character – Upper Floor (Level 12)	Approval
EBE-AST-XX-XX-DR-A-011150	Access and Circulation	Approval
EBE-AST-XX-XX-DR-A-011160	External Amenity – Ground Floor	Approval
EBE-AST-XX-XX-DR-A-011161	External Amenity – Podium	Approval
EBE-AST-XX-XX-DR-A-011161	External Amenity – Terraces	Approval
Detailed Area Architectural Plans – Blocks 7 and 8 (EBR-DRG-DA)		
EBE-AST-XX-XX-DR-A-012000	Existing Site Location Plan	Information
EBE-AST-XX-XX-DR-A-012001	Proposed Site Location Plan	Approval
EBE-AST-XX-XX-DR-A-012109	Proposed GA Plan – Basement	Approval
EBE-AST-XX-XX-DR-A-012110	Proposed GA Plan – Ground	Approval
EBE-AST-XX-XX-DR-A-012111	Proposed GA Plan – First Floor	Approval
EBE-AST-XX-XX-DR-A-012112	Proposed GA Plan – Second Floor	Approval
EBE-AST-XX-XX-DR-A-012113	Proposed GA Plan – Third Floor	Approval
EBE-AST-XX-XX-DR-A-012114	Proposed GA Plan – Fourth Floor	Approval
EBE-AST-XX-XX-DR-A-012115	Proposed GA Plan – Fifth Floor	Approval
EBE-AST-XX-XX-DR-A-012116	Proposed GA Plan – Sixth Floor	Approval

EBE-AST-XX-XX-DR-A-012117	Proposed GA Plan – Seventh Floor	Approval
EBE-AST-XX-XX-DR-A-012118	Proposed GA Plan – Eighth Floor	Approval
EBE-AST-XX-XX-DR-A-012119	Proposed GA Plan – Ninth Floor	Approval
EBE-AST-XX-XX-DR-A-012120	Proposed GA Plan – Tenth Floor	Approval
EBE-AST-XX-XX-DR-A-012121	Proposed GA Plan – Eleventh Floor	Approval
EBE-AST-XX-XX-DR-A-012122	Proposed GA Plan – Twelfth Floor	Approval
EBE-AST-XX-XX-DR-A-012123	Proposed GA Plan – Thirteenth Floor	Approval
EBE-AST-XX-XX-DR-A-012124	Proposed GA Plan – Fourteenth Floor	Approval
EBE-AST-XX-XX-DR-A-012125	Proposed GA Plan – Fifteenth Floor	Approval
EBE-AST-XX-XX-DR-A-012126	Proposed GA Plan – Sixteenth Floor	Approval
EBE-AST-XX-XX-DR-A-012127	Proposed GA Plan – Roof	Approval
EBE-AST-XX-XX-DR-A-012150	Tenure Plan – Building 7	Approval
EBE-AST-XX-XX-DR-A-012151	Tenure Plan – Building 8	Approval
EBE-AST-XX-XX-DR-A-012200	Section AA: Detailed Area Section – North West	Approval
EBE-AST-XX-XX-DR-A-012201	Detailed Area Section – Building 7 – South West	Approval
EBE-AST-XX-XX-DR-A-012202	Detailed Area Section – Building 8 – South West	Approval
EBE-AST-XX-XX-DR-A-012300	Detailed Area – North West Elevation	Approval
EBE-AST-XX-XX-DR-A-012301	Detailed Area – South East Elevation	Approval
EBE-AST-XX-XX-DR-A-012302	Detailed Area – North East Elevation	Approval
EBE-AST-XX-XX-DR-A-012303	Detailed Area – South West Elevation	Approval
EBE-AST-XX-XX-DR-A-012400	Bay Study – South West Elevation	Approval
EBE-AST-XX-XX-DR-A-012401	Bay Study – North East Elevation	Approval
EBE-AST-XX-XX-DR-A-012402	Bay Study – South East Elevation	Approval
EBE-AST-XX-XX-DR-A-012403	Bay Study – North West Elevation	Approval
EBE-AST-XX-XX-DR-A-012404	Building 7 – Roof Expression	Approval
EBE-AST-XX-XX-DR-A-012405	Building 8 – Roof Expression	Approval
EBE-AST-XX-XX-DR-A-012500	Typical Balustrade Design	Approval
Detailed Area Landscape Plans – Blocks 7 and 8 (EBR-DRG-DL)		
EBE-LBA-XX-GF-DR-L-300011	Landscape Ground Floor (rendered)	Approval

EBE-LBA-XX-GF-DR-L-30 0012	Landscape First Floor Podiums (rendered)	Approval
EBE-LBA-XX-GF-DR-L-30 0013	Landscape Roof Terraces (rendered)	Approval
EBE-LBA-XX-GF-DR-L-30 0014	Landscape Ground Floor GA	Approval
EBE-LBA-XX-GF-DR-L-30 0015	Landscape First Floor Podiums GA	Approval
EBE-LBA-XX-GF-DR-L-30 0016	Landscape Roof Terraces GA	Approval
EBE-LBA-XX-GF-DR-L-30 0017	Planting Schedules	Approval
EBE-LBA-XX-GF-DR-L-30 0018	Planting Plan Ground Floor	Approval
EBE-LBA-XX-GF-DR-L-30 0019	Planting Plan First Floor Podiums	Approval
EBE-LBA-XX-GF-DR-L-30 0020	Planting Plan Roof Terraces	Approval
EBE-LBA-XX-GF-DR-L-30 0021	Landscape Sections	Approval
EBE-LBA-XX-GF-DR-L-30 0022	Landscape Details	Approval